

CONTENTS	
1.0	Application of Policy
2.0	Relationship to other Policies and Guidelines
3.0	Context
	3.1 Precinct Map 3.2 Policy Objectives 3.3 Desired Future Character Statement
4.0	General Provisions
	4.1 Policy Objectives and Desired Future Character Statements 4.2 Sustainability 4.3 Public Open Space 4.4 Landscaping 4.5 Public Domain Interface 4.6 Urban Design and Materiality 4.7 Subdivision 4.8 Vehicle Access 4.9 Car and Bicycle Parking 4.10 Noise Mitigation
5.0	Built Form Provisions
	5.1 Residential Sub-Precinct (R40, R60, R160) 5.1.1 Existing Character Statement 5.1.2 Desired Future Character Statement 5.1.3 Deemed-to-comply requirements and Acceptable Outcomes A. Residential R40 B. Residential R60 C. Residential R160 5.1.4 Design Guidance 5.1.5 Housing Objectives 5.2 Mixed Use Sub-Precinct (R-AC3) 5.2.1 Existing and Desired Future Character Statements A. Hampden Road (R-AC3) B. Monash Avenue (R-AC3) C. Leura Street (R-AC3) 5.2.2 Acceptable Outcomes 5.2.3 Design Guidance
6.0	Definitions
7.0	Appendices
	Appendix 1 – Sustainability and Energy Efficiency Initiatives Appendix 2 – Facades and Materials Appendix 3 - Parking Requirements - Mixed Use Zone (Preferred Active Land Uses)
<p>Note: Elements of this Policy which have been struck out are awaiting approval by the WAPC and are not operative.</p>	

1 APPLICATION OF POLICY

- 1.1** This Policy applies to the Hampden-Hollywood Precinct as identified in Figure 1.
- 1.2** This Policy applies to subdivision applications and development applications for Single Houses, Grouped Dwellings, Multiple Dwellings, Mixed Use development and non-residential development.

2 RELATIONSHIP TO OTHER POLICIES AND GUIDELINES

- 2.1** This Policy has been prepared in accordance with Schedule 2 Part 2 Clause 4 of the *Planning and Development (Local Planning Schemes) Regulations 2015*.
- 2.2** This Policy should be read in conjunction with the following planning instruments and its requirements apply unless specifically stipulated elsewhere in any of the below:
- *State Planning Policy 7.3 – Residential Design Codes Volume 1*
 - *State Planning Policy 7.3 – Residential Design Codes Volume 2 – Apartments*
 - *State Planning Policy 7.2 – Precinct Design*
 - *City of Nedlands Local Planning Scheme No. 3*
 - *City of Nedlands Local Planning Policy – Residential Development*
- 2.3** Where this Policy is inconsistent with a Local Development Plan that applies to a specific site, area, or density code, the provisions of that Local Development Plan shall prevail over this Policy.
- 2.4** Where this Policy is inconsistent with the provisions of another general Local Planning Policy, the provisions of this Policy shall prevail.

3 CONTEXT

3.1 PRECINCT MAP



Figure 1 – Precinct Map

3.2 POLICY OBJECTIVES

1. Clearly outline the desired future character of the Hampden-Hollywood Precinct in context with its zoning, density coding and in consideration of its function within the UWA-QEII Specialised Activity Centre.
2. Ensure that new development contributes to the desired future character of the Hampden-Hollywood Precinct, while respecting and reflecting existing character.
3. Provide appropriate built form transitions between areas of higher density and areas of lower density.
4. Facilitate housing diversity appropriate to the needs of the local community, and to encourage a permanent population, ageing in place, and more housing for students and hospital staff.
5. Facilitate high-quality development which maximises residential amenity and liveability.
6. Maintain and enhance the leafy green landscape character of the Hampden-Hollywood Precinct, promoting growth of urban forest through tree canopies in yard spaces and setbacks, and tree canopies along the roads.
7. Promote active living towards the front of the street and onto laneways, providing variety, articulation and interest at street level and from the public realm.
8. Ensure the provision of setbacks creates a consistent streetscape and built form, and ensure buildings co-exist with neighbouring properties.
9. Optimise energy and water efficiency of new developments through sustainable building design.
10. Reduce reliance on private vehicles and encourage alternate modes of transport.

3.3 DESIRED FUTURE CHARACTER STATEMENT

New development and associated land uses within the Hampden-Hollywood Precinct will respond to their location within the UWA-QEII Specialised Activity Centre, while also catering to the needs of the local community. The Precinct will provide diverse housing options for residents and the medical-related workforce, including dwellings for families, young professionals, and provision for ageing in place. Housing will be within high amenity and attractive streetscapes, retaining the Precinct's leafy green character by enhancing and prioritising tree canopy and **landscaping** in new developments.

Development within the Residential zone will include elements that positively contribute to the existing residential character using a high-quality palette of diverse, sustainable materials and architectural styles. New homes will be encouraged to address the street and laneways with courtyards, balconies, low fences and front gates to reinforce the residential character and promote passive surveillance.

Built form and **landscaping** will be designed to provide appropriate transitions from areas of higher density to areas of lower density. Architecture and building forms shall respond to the evolving local context, reflecting elements of neighbourhood character including (but not limited to) leafy, green streetscapes, space for mature trees to grow within attractive gardens, and a variety of building materials.

New developments within the Mixed Use zone will contribute to the creation of attractive and green streetscapes and interact with the street to enhance the pedestrian environment. Land uses on the ground floor within this zone will connect the private and public realms, interacting with the street through open and unobscured frontages and/or alfresco dining options. The Mixed Use zone will accommodate **active land uses** at street level (such as cafés, restaurants and shops) with non-active commercial (such as offices) and apartments above. Demarcation between the private and public realms will enhance the pedestrian environment on Hampden Road, Monash Avenue and Leura Street, promoting an active, vibrant and liveable local centre.

4 GENERAL PROVISIONS
4.1 Policy Objectives and Desired Future Character Statements
4.1.1 All development applications within the Hampden-Hollywood Precinct shall be consistent with the Objectives of this Policy and the applicable Desired Future Character Statement.
4.2 Sustainability (Note: Awaiting approval from WAPC for 4.2.3 - 4.2.6 to apply)
4.2.1 All developments with a commercial Gross Floor Area greater than 1000m ² shall be designed and constructed to achieve a minimum rating of 5 Green Stars under the Green Building Council of Australia Green Star rating tool as certified by an accredited assessor.
4.2.2 All commercial development shall be designed to achieve and maintain minimum NABERS rating of 5.5 Stars.
4.2.3 The following sustainability measures are required for Single House and Grouped Dwelling developments: <ul style="list-style-type: none"> i. When fittings and appliances are to be within one level of the highest level available under the Water Efficiency Labelling and Standards (WELS) system; and ii. Incorporate at least one significant energy efficiency initiative within the development that exceeds minimum practice (refer Appendix 1); OR iii. All dwellings exceed the minimum NatHERS requirements by 1 star.
4.2.4 For all development applications proposing Grouped Dwellings and/or Multiple Dwellings, a sustainability report, completed by a suitably qualified sustainability consultant must be provided. This report must demonstrate how the requirements of section 4.2.3 have been addressed. The efficiencies demonstrated will be required to be implemented as part of any development approval.
4.2.5 For all Mixed Use, Grouped Dwelling, and Multiple Dwelling development applications, electrical vehicle charging is to be provided at a minimum rate of 50 per cent of total residential bays. Where this charging infrastructure has not been provided, electrical supply and car park distribution boards are to allow for future capacity to supply electric vehicle charging points to the remainder of the bays.
4.2.6 New developments are to consider selecting building materials based on suitable thermal mass and lifecycle costs.
4.3 Public Open Space
4.3.1 In accordance with <i>Development Control Policy 2.3 – Public Open Space in Residential Areas</i> , a Public Open Space contribution of 10 per cent of the gross residential area or cash-in-lieu of the equivalent value, shall be required for all subdivision applications (including strata applications) where 6 or more residential lots are created, unless otherwise stated in a specific public open space policy.
4.4 Landscaping (Note: Awaiting approval from WAPC for 4.4.1 – 4.4.8 to apply)
4.4.1 All new developments are to demonstrate the principles of Water Sensitive Urban Design, including: <ul style="list-style-type: none"> i. Maximising the use of permeable surfaces at ground level to enable groundwater recharge, and minimising impervious areas; ii. Incorporating on-site infiltration and detention systems such as garden beds, rain gardens, tree pits, infiltration cells and detention tanks (the latter shall be sited to avoid conflict with deep soil areas); and iii. Designing landscape treatments to slow down overland flows and minimise scouring.
4.4.2 Except for heavily shaded areas, species selection shall prioritise the use of endemic and native species, with an emphasis on drought tolerance and provision of shade. See the City's Sustainable Landscaping Advice Information Sheet for suggested species.
4.4.3 On-structure planting is encouraged in addition to the minimum deep soil areas and tree canopy requirements. Where on-structure planting is proposed, the structure must be designed to provide suitable drainage to root systems and avoid the pooling of water.
4.4.4 Where on-structure planting is proposed (including planting above a basement level), the

Local Planning Policy 5.11: Hampden-Hollywood Precinct

<p>landscaping plan provided with the Development Application must detail:</p> <ul style="list-style-type: none"> i. The proposed planting design, including planter box widths, depths, water supply & drainage. ii. Suitability of plants to ensure on structure planting is viable as a long-term greening option. iii. Reticulation and maintenance by the strata body. 										
<p>4.4.5 Trees and deep soil areas specified throughout this Policy are to be provided in accordance with the below and demonstrated in a landscaping plan:</p>										
Tree size	Indicative canopy diameter at maturity	Indicative height at maturity	Minimum width of DSA dimensions per tree	Minimum planting area per tree						
Small	4-6m	4-8m	2m x 2m	9m ²						
Medium	6-9m	8-12m	3m x 3m	36m ²						
Large	>9m	>12m	6m x 6m	64m ²						
<p>4.4.6 Deep soil areas require a minimum width of 2m. This may be reduced to 1.5m where it adjoins rootable soil zones with a minimum dimension of 1m (not including soil beneath built structures) OR where it adjoins permeable paving with a minimum of 0.5m.</p>										
<p>4.4.7 Deep soil areas are to be located against the parent lot boundaries where possible.</p>										
<p>4.4.8 Artificial turf is not to be visible from the public realm. Artificial turf will not be considered as contributing to deep soil area.</p>										
<p>4.5 Facades and Materials</p>										
<p>4.5.1 The facades and materials of new development are to reference the exiting facades and materials, and key design elements, for each sub-precinct, as set out in Appendix 2.</p>										
<p>4.5.2 To reduce the urban heat island effect, and to integrate with the prevailing streetscape, roof materials of all new developments are to have the following maximum solar absorptance ratings (<i>Photovoltaic panels or similar are exempt from this provision</i>):</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 60%;">Roof Structure</th> <th>Maximum Solar Absorptance Rating</th> </tr> </thead> <tbody> <tr> <td>Flat roof structures that are not visible for the street or adjacent properties</td> <td>0.4</td> </tr> <tr> <td>Pitched roof structures or roof structures that are visible from the street or adjacent properties</td> <td>0.5</td> </tr> </tbody> </table>					Roof Structure	Maximum Solar Absorptance Rating	Flat roof structures that are not visible for the street or adjacent properties	0.4	Pitched roof structures or roof structures that are visible from the street or adjacent properties	0.5
Roof Structure	Maximum Solar Absorptance Rating									
Flat roof structures that are not visible for the street or adjacent properties	0.4									
Pitched roof structures or roof structures that are visible from the street or adjacent properties	0.5									
<p>4.6 Public Domain Interface</p>										
<p>4.6.1 Where a development adjoins a laneway, a minimum of one major opening, balcony, or terrace is required to overlook the laneway for passive surveillance. One pedestrian access point is also required.</p>										
<p>4.6.2 Solid portions of Secondary Street fences should be a maximum height of 1.5m where that part of the fence is dividing the resident's primary open space areas from the street.</p>										
<p>4.6.3 Where a fence occurs on a structure that is raised above the adjacent street level (e.g., where there is a half-basement), the fence is to be visually permeable above 1.2m from the street level.</p>										
<p>4.6.4 Where facing a primary or secondary street or laneway, balcony balustrades are to be a minimum of 50 per cent visually permeable.</p>										
<p>4.7 Subdivision</p>										
<p>4.7.1 Where a laneway provides access to a site, vehicle access to the rear site is to be from the laneway. Pedestrian access is to be provided from the rear site to the primary street by a pedestrian access leg.</p>										
<p>4.7.2 Lot amalgamation is encouraged to create development efficiencies and to facilitate significant existing tree retention.</p>										
<p>4.8 Vehicle Access – Multiple dwellings (Note: Awaiting approval from WAPC to apply to single houses and grouped dwellings)</p>										

Local Planning Policy 5.11: Hampden-Hollywood Precinct

4.8.1	Driveways serving four dwellings or less are not to be narrower than 3m at the street boundary.
4.8.2	Where vehicles are required to enter the street in forward gear in accordance with A3.8.6 of the R-Codes Vol. 2, this is to be facilitated through passing points within the lot boundaries.
4.8.3	A maximum of one Primary Street crossover is permitted per development site, including amalgamated lots.
4.8.4	Vehicle access is to be designed and located to avoid the removal of street trees.
4.8.5	Where a communal street is proposed, all proposed dwellings are to take vehicle access from that communal street.
4.8.6	Where a laneway, 4m in width or greater, provides suitable access to a site primary vehicle access is to be taken from the laneway.
4.9	Car and Bicycle Parking (Note: Awaiting approval from WAPC for 4.9.4 to apply)
4.9.1	For the purpose of assessing car parking requirements under 5.3.3 of the R-Codes Volume 1 and 3.9 of the R-Codes Volume 2, the Hampden-Hollywood Precinct is to be classified as Location A. This classification is reflective of the Precinct being located within the UWA-QEII Specialised Activity Centre, and the presence of the Purple CAT (Central Area Transit) bus route along Hampden Road, which provides frequent, free connections between Elizabeth Quay Bus Station, QEII Medical Centre and the University of Western Australia.
4.9.2	In Multiple Dwelling and Mixed Use developments, a reduction in the number of residential car bays may be considered where electric vehicles are provided for shared use. Where proposed, a parking management plan may be required to be submitted as part of the application.
4.9.3	In accordance with Clause 2.3 of the City of Nedlands <i>Local Planning Policy - Parking</i> , non-residential parking requirements are outlined in Appendix 3 of this Policy.
4.9.4	At ground or above-ground car parking (excluding visitor parking) is to be sleeved behind other land uses, or other portions of the building, along the street frontage.
4.10	Noise Mitigation
4.10.1	All applications for development approval that include residential dwellings must demonstrate how the development responds to and accommodates anticipated noise pollution from the QEII Hospital Precinct.

5 BUILT FORM PROVISIONS

All development within the Residential zone must be consistent with the Desired Future Character Statement for the relevant sub precinct. The Acceptable Outcomes, Design Guidance and Housing Objectives specific to each density code provide further contextual guidance for applicants.

5.1 RESIDENTIAL SUB-PRECINCT (R40, R60, R160)

5.1.1 Existing Character Statement

The Residential sub-precinct generally comprises one and two storey single residential dwellings on lots with substantial open space, with the occasional three storey house. Streetscapes are characterised by generous front setbacks, with well-kept, green, and often native domestic gardens. The trees and vegetation within front yards and on verges strongly contribute to the leafy green character of the Precinct, creating a shady, attractive pedestrian environment.

Streetscapes are predominantly detached in nature, with dwellings, verandahs and open car ports oriented to the street. Side setbacks are generally 0 -1 metres from the boundary, creating a comfortable separation between buildings. The majority of lots are considered 'fine grain development' due to the small street frontage of the single houses.

There is a variety of traditional architectural styles throughout the sub-precinct, including Federation Cottage's and Californian Bungalow's. The materials associated with these dwellings are typically rich red brick and terracotta tile, with off white and cream features. Roof forms are generally hipped and tiled or corrugated iron. Rough rendered and painted walls, bands of face brick below the eaves, and timber-framed windows are also prominent. Entries, verandahs and windows provide passive surveillance of the street. Where front fencing is present, it is typically of a low and open style that demarcates the private and public realm but maintains passive surveillance of the street. On-site vehicle parking is characterised by a mix of open car spaces, carports and garages. Enclosed garages facing the Precinct's laneways are also common.

5.1.2 Desired Future Character Statement

The Residential sub-precinct will provide for a diversity of dwelling types and configurations, to accommodate a range of demographics and household types. New dwellings will perform an important role in transitioning from the high-density urban environment expected along Hampden Road, Monash Avenue and Leura Street, towards the existing lower density Residential R10 and R12.5 areas to the west.

New developments should fit comfortably within the existing leafy streetscapes, achieved through generous **street setbacks**, striking a balance between the prevailing **street setbacks** and the expectation of the density code. Street and rear (laneway) setbacks will accommodate **deep soil areas** for tree retention and tree provision. Vehicle access and parking will be designed to minimise visual impact to the street and maximise space for **deep soil areas** and trees within the **street setback** area and verge.

Substantial areas of **landscaping** will be integrated with new developments to soften the interface with the street and surrounding properties, and to provide an attractive outlook for residents of the developments. New developments will maintain and enhance the existing urban tree canopy, across both the private and public realms. The existing detached streetscape character will be retained by minimising the impact of boundary walls on the street and by providing a large consistent setback to one side of the developments.

New developments will provide a contemporary interpretation of the prevailing streetscape elements and materiality. A range of high-quality materials and architectural styles are encouraged to maintain the varied nature of development within this sub-precinct.

5.1.3 Deemed-to-comply requirements and Acceptable Outcomes (Residential Sub-Precinct)

R40 -The R40 portion of the Residential sub-precinct comprises most of the Hampden-Hollywood Precinct area, and is bound by Williams Road to the west, Gordon Street to the south, Drosera Lane and R60 lots to the east and Hardy Road to the north. This sub-precinct will provide a transition from the higher densities within the Precinct to the established residential areas coded 12.5 to the west.

SINGLE HOUSES AND GROUPED DWELLINGS (R40)

DC	Primary Control	Deemed-to-comply requirement
DC 1.1	Building Height	Maximum 2 storeys (8.5m wall or concealed roof height, 10m pitched roof height)
DC 1.2	Primary Street setback ^{2,4,6}	<u>Ground floor to 2nd storey</u> Minimum 4m
DC 1.3	Secondary Street or Communal Street setback ^{2,4,5,6,}	Minimum 1.5m
DC 1.4	Side setbacks ^{2,4,6} (Note: Awaiting approval from WAPC for DC 1.4 to apply)	<u>Northern lot boundary</u> Minimum 3m <u>Southern lot boundary (ground floor)</u> Minimum nil <u>Southern lot boundary (2nd storey)</u> Minimum 1.5m
DC 1.5	Rear setback ^{2,4,6} (Note: Awaiting approval from WAPC for DC 1.5 to apply)	Minimum 3m
DC 1.6	Lot Boundary Walls ²	<ul style="list-style-type: none"> • Maximum 1 storey (3.5m) in height • 2 storeys (7m) for internal lot boundaries⁷. • Permitted to the southern side boundary of the parent lot and internal lot boundaries only. • Located a minimum of 3m behind the primary street setback.

MULTIPLE DWELLINGS (R40)

AO	Primary Control	Acceptable Outcome
AO 1.1	Building Height	Maximum 2 storeys (9m)
AO 1.2	Primary Street setback ^{2,4,6}	Minimum 4m (Ground floor to 2 nd storey)
AO 1.3	Secondary Street or Communal Street setback ^{2,4,5,6,}	Minimum 1.5m
AO1.4	Side setbacks ^{2,4,6}	<u>Northern lot boundary</u> Minimum 3m <u>Southern lot boundary (ground floor)</u> Minimum nil <u>Southern lot boundary (2nd storey)</u> Minimum 1.5m
AO 1.5	Rear setback ^{2,4,6}	Minimum 3m
AO 1.6	Lot Boundary Walls ²	<ul style="list-style-type: none"> • Maximum 1 storey (4m) in height • 2 storeys (7m) for internal lot boundaries⁷. • Permitted to the southern side boundary of the parent lot and internal lot boundaries only. • Located a minimum of 3m behind the Primary Street setback line

¹ Subject to indicative building heights outlined in Table 2.2 of R-Codes Vol. 2. Maximum overall height inclusive of rooftop articulation.

² Boundary setbacks will also be determined by provisions for building separation, **deep soil areas** and visual privacy within R-Codes Vol. 1, Vol. 2 and this Policy and building separation provisions of the National Construction Codes.

³ Subject to the provision of half basement car parking, maximum 2.5 storeys.

⁴ Reduced setbacks may be considered where a **significant existing tree** is retained within that setback area or elsewhere on site.

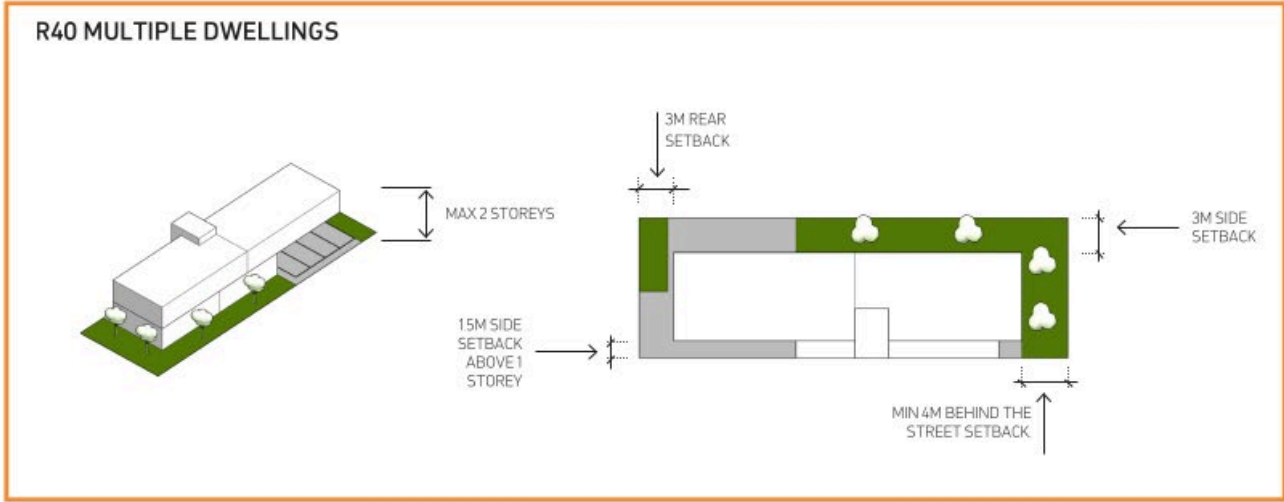
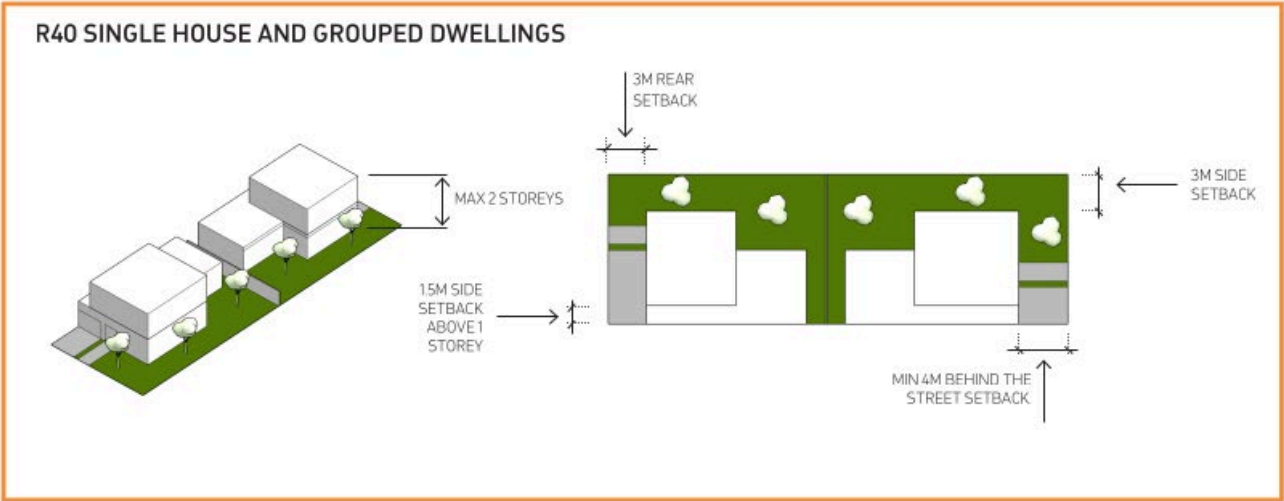
⁵ A Nil setback to a communal street or internal driveway may be considered where DSA is provided elsewhere within the communal street setback area.

Local Planning Policy 5.11: Hampden-Hollywood Precinct

⁶ Unwalled carports are supported in the front setback area, and at a nil-setback to one side of the lot, where the carport's car spaces align with the crossover.

⁷ Subject to indicative building heights outlined in Table 2.2 of R-Codes Vol. 2, excluding 2m provision for rooftop articulation.

R40 ZONE



Draft Hampden-Hollywood Precinct Design Response - Local Planning Policy

R60 - The R60 portion of the Residential sub-precinct is south of Gordon Street (north of R160 lots), south of Micrantha Lane (north of Hardy Road) and west of Leura Street (east of Drosera Lane). It provides an important transition between the higher density areas of R160 to the south, R-AC3 to the north and east, and the R40 areas.

SINGLE HOUSES AND GROUPED DWELLINGS (R60)

DC	Primary Control	Deemed-to-comply requirement
DC 2.1	Building Height	Maximum 2 storeys (8.5m wall or concealed roof height, 10m pitched roof height).
DC 2.2	Primary Street setback ²	Minimum 4m (Ground floor to 2 nd storey)
DC 2.3	Secondary or Communal Street setback ²	Minimum 1.5m
DC 2.4	Side setbacks ^{2,3} (Parent lot oriented east/west) (Note: Awaiting approval from WAPC for DC 2.4 to apply)	<u>Northern lot boundary</u> Minimum 3m <u>Southern lot boundary (ground floor)</u> Minimum nil <u>Southern lot boundary (2nd storey and above)</u> Minimum 1.5m
DC 2.5	Side setbacks ^{2,3} (Parent lot oriented north/south) (Note: Awaiting approval from WAPC for DC 2.5 to apply)	<u>Western lot boundary</u> Minimum 3m <u>Eastern lot boundary (ground floor)</u> Minimum nil <u>Eastern lot boundary (2nd storey and above)</u> Minimum 1.5m
DC 2.6	Rear setback ² (Note: Awaiting approval from WAPC for DC 2.6 to apply)	Minimum 3m
DC 2.7	Lot Boundary Walls ^{4, 8}	<ul style="list-style-type: none"> • Maximum 1 storey (3.5m) in height • Permitted to the southern lot boundary (east/west oriented lots) or eastern lot boundary (north/south oriented lots) only • Located a minimum of 2m behind the Primary Street setback line

MULTIPLE DWELLINGS (R60)

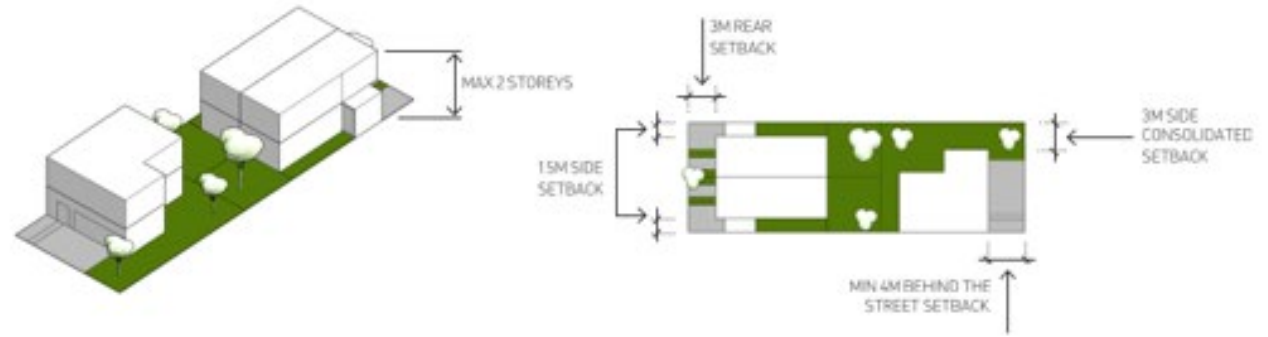
AO	Primary Control	Acceptable Outcome
AO 2.1	Building Height ¹	Maximum 3 storeys (12m)
AO 2.2	Primary Street setback ²	<u>Ground floor and 2nd storey</u> Minimum 4m <u>3rd storey and above</u> Minimum 5m
AO 2.3	Secondary or Communal Street setback ²	Minimum 1.5m
AO 2.4	Side setbacks ^{2,3} (Parent lot oriented east/west)	<u>Northern lot boundary</u> Minimum 3m <u>Southern lot boundary (ground floor)</u> Minimum nil <u>Southern lot boundary (2nd storey and above)</u> Minimum 1.5m
AO 2.5	Side setbacks ^{2,3} (Parent lot oriented north/south)	<u>Western lot boundary</u> Minimum 3m <u>Eastern lot boundary (ground floor)</u> Minimum nil <u>Eastern lot boundary (2nd storey and above)</u> Minimum 1.5m
AO 2.6	Rear setback ²	Minimum 3m

<p>AO 2.7</p>	<p>Lot Boundary Walls^{4,8}</p>	<ul style="list-style-type: none"> • Maximum 1 storey (4m) in height • Permitted to the southern lot boundary (east/west oriented lots) or eastern lot boundary (north/south oriented lots) only • Located a minimum of 2m behind the Primary Street setback line
<p>¹ Subject to indicative building heights outlined in Table 2.2 of R-Codes Vol. 2. Maximum overall height inclusive of rooftop articulation.</p> <p>² Boundary setbacks will also be determined by provisions for building separation and visual privacy within R-Codes Vol. 1, Vol. 2 and this LPP and building separation provisions of the National Construction Codes.</p> <p>³ If the parent lot is amalgamated, the required side setback is a minimum 6m. (Note: Awaiting approval from WAPC for footnote 3 to apply)</p> <p>⁴ Where the subject site and an adjoining site are subject to different density codes, the length and height of any boundary wall is determined by reference to the lower density code.</p> <p>⁵ Reduced setbacks may be considered where a significant existing tree is retained within that setback area or elsewhere on site.</p> <p>⁶ A Nil setback to a communal street or internal driveway may be considered where DSA is provided elsewhere within the communal street setback area.</p> <p>⁷ Unwalled carports are supported in the front setback area, and at a nil-setback on one side of the lot, where the carport's car spaces align with the crossover.</p> <p>⁸ May be built up to a lot boundary, where it abuts an existing or simultaneously constructed wall of equal or greater proportions.</p>		

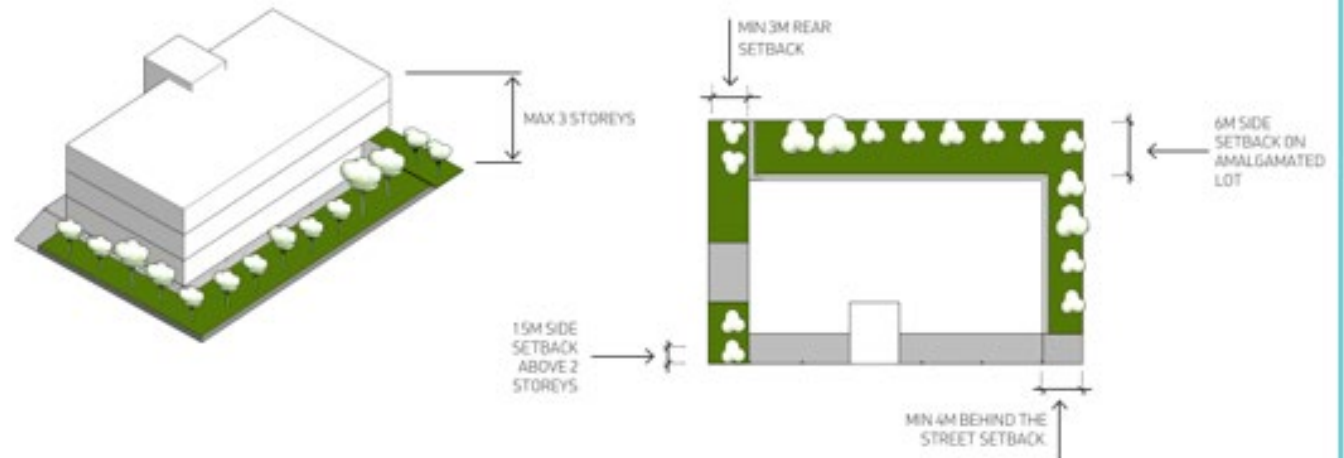
R60 ZONE



R60 SINGLE HOUSE AND GROUPED DWELLINGS



R60 MULTIPLE DWELLINGS



Draft Hampden-Hollywood Precinct Design Response - Local Planning Policy

R160 - The R160 portion of the Precinct comprises the northern side of Hibbertia Lane, west of Clifton Street, east of Williams Road and adjoining the R60 zoned lots south of William Street. This sub-precinct provides an important transition between the higher density areas of R-AC1 adjacent to Stirling Highway, and R60 properties to the north.

SINGLE HOUSES AND GROUPED DWELLINGS (R160)

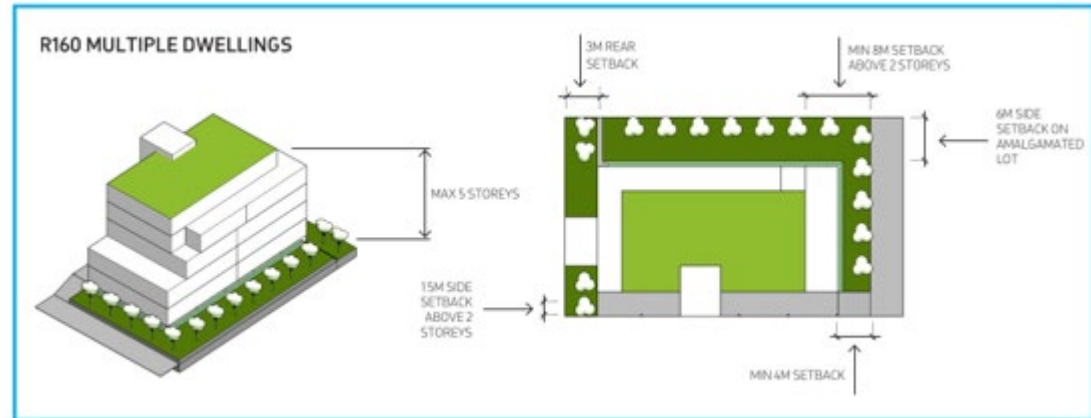
DC	Primary Control	Deemed-to-comply requirement
DC 3.1	Building Height	Maximum 2 storeys (8.5m wall or concealed roof height, 10m pitched roof height)
DC 3.2	Primary Street setback ^{2,5}	Minimum 4m (Ground floor to 2 nd storey)
DC 3.3	Secondary Street setback (Hibbertia Lane) or Communal Street ^{2,5,6}	Minimum 1.5m
DC 3.4	Side setbacks ^{2,3,5} (Note: Awaiting approval from WAPC for DC 3.4 to apply)	<u>Northern lot boundary</u> Minimum 3m <u>Southern lot boundary (ground floor)</u> Minimum nil <u>Southern boundary (2nd storey and above)</u> Minimum 1.5m
DC 3.5	Rear setback ^{2,5} (Note: Awaiting approval from WAPC for DC 3.5 to apply)	Minimum 3m
DC 3.6	Boundary Walls ^{2,3,7}	<ul style="list-style-type: none"> • Maximum 1 storey (3.5m) in height • Maximum 2 storeys (7m) for internal lot boundaries⁸ • Permitted to southern boundary of parent lot only • Located a minimum of 2m behind the Primary Street setback line

MULTIPLE DWELLINGS (R160)

AO	Primary Control	Acceptable Outcome
AO 3.1	Building Height ¹	Maximum 5 storeys (18m)
AO 3.2	Primary Street setback ^{2,5}	<u>Ground floor to 2nd storey</u> Minimum 4m <u>3rd storey and above</u> Minimum 6m
AO 3.3	Secondary Street setback (Hibbertia Lane) or Communal Street ^{2,5,6}	Minimum 1.5m
AO 3.4	Side setbacks ^{2,3,5}	<u>Northern lot boundary</u> Minimum 3m <u>Southern lot boundary (ground floor)</u> Minimum nil <u>Southern boundary (2nd storey and above)</u> Minimum 1.5m
AO 3.5	Rear setback ^{2,5}	<u>Ground floor to 3rd storey</u> Minimum 3m <u>4th storey and above</u> Minimum 6m
AO 3.6	Lot Boundary Walls ^{2,3,7}	<ul style="list-style-type: none"> • Maximum 1 storey (4m) in height • Permitted to southern side boundary of parent lot only • Located a minimum of 2m behind the Primary Street setback line

- ¹ Subject to indicative building heights outlined in Table 2.2 of R-Codes Vol. 2. Maximum overall height inclusive of rooftop articulation.
- ² Boundary setbacks will also be determined by provisions for building separation and visual privacy within R-Codes Vol. 1, Vol. 2 and this LPP and building separation provisions of the National Construction Codes.
- ³ ~~If the parent lot is amalgamated, a minimum 6m setback is required to the northern lot boundary.~~ **(Note: Awaiting approval from WAPC for Footnote 3 to apply)**
- ⁴ Where the subject site and an affected adjoining site are subject to different density codes, the length and height of any boundary wall between them is determined by reference to the lower density code.
- ⁵ Reduced setbacks may be considered where a **significant existing tree** is retained within that setback area or elsewhere on site.
- ⁶ A Nil setback to a communal street or internal driveway may be considered where DSA is provided elsewhere within the communal street setback area.
- ⁷ May be built up to a lot boundary, where it abuts an existing or simultaneously constructed wall of equal or greater proportions.
- ⁸ Subject to indicative building heights outlined in Table 2.2 of R-Codes Vol. 2, excluding 2m provision for rooftop articulation.

R160 ZONE



5.1.4 Multiple Dwellings – Acceptable Outcomes and Design Guidance

Acceptable Outcomes (AO)

In accordance with section 1.2.2 and 1.2.3 of the R-Codes Vol. 2, the below provisions amend or replace acceptable outcome provisions in the R-Codes Vol. 2.

Design Guidance (DG)

Design Guidance provides additional direction for applicants to ensure that proposals are contextually appropriate for the specific sub-precinct.

2.4 Side and rear setbacks

a) Lot boundary walls are permitted on the southern parent lot boundary (for R40, R60 and R160 lots oriented east/west) and the western parent lot boundary (for R60 lots oriented north/south) to provide a useable consolidated area of open space on one side of the site.

3.3 Tree canopy and deep soil areas - (Note: Awaiting approval from WAPC for Acceptable Outcomes to apply)

~~a) Any new development is to include the following minimum number of trees (per parent lot or amalgamated parent lots being developed):~~

Site Area	Minimum requirement for trees
<700m ²	2 medium trees AND small trees to suit area
700 – 1000m ²	3 medium trees AND Small trees to suit area OR 1 large tree AND small trees to suit area
>1000m ²	In addition to the above; 1 large tree OR 2 medium trees OR 5 small trees for each additional 400m ² in excess of 1000m ² .

a) **Deep soil areas** within the street setback area should be co-located with apartment private open space.

b) Where hard surfaces are proposed within outdoor living areas or adjacent to **deep soil areas**, permeable surfaces are encouraged to reduce stormwater run-off and allow infiltration to rootable soil zones.

~~b) Of the trees required above, a minimum of 1 medium tree or 1 large tree is required to be planted and maintained in each street setback area for each street facing dwelling, and each rear setback area (either within private or communal open space) for each laneway facing dwelling. Small trees are required to be provided for internal lots or central areas of the site.~~

~~c) Where a medium tree or large tree is proposed within the street setback area, the minimum tree planting area for that tree may project into the verge by 40%. **Deep soil area** is to be on site and may not project into the verge.~~

d) ~~The front and rear setbacks areas shall be utilised for the inclusion of **deep soil areas** and tree plantings. This allows for a landscaped buffer between the proposed development and the rear adjoining site/s, softening the interface with the street. Where possible, **deep soil areas** should be located against lot boundaries.~~

e) ~~Upper floors or balconies of buildings may extend over portions of Landscape Area (up to a maximum of 20% of that area), subject to a landscaping plan demonstrating the proposed species are capable of long-term growth within a shaded environment. This does not apply to south facing elevations.~~

f) ~~A minimum total of 20% of the site area (area of parent lot/s) is to be provided as **landscaping**. This total shall include at least 15% of the site area to be **deep soil area**.~~

g) ~~The required **deep soil area** may be reduced to 10% of the site area if a **significant existing tree** is retained on site, or if a large tree is planted on site.~~

3.8 Vehicle access

- a) The following can reduce the visual impact of vehicle access and dominance at the street frontage:
- i. Keeping crossovers a functional minimal width to maximise space in the street setback area and verge for **deep soil areas** and tree retention / provision.
 - ii. Providing sole vehicle access from a laneway or communal street.
 - iii. Alternative colours/treatments of crossovers i.e., not asphalt.

5.1.5 Single Houses and Grouped Dwellings – Deemed to Comply Provisions and Housing Objectives

Deemed to Comply (DC)

In accordance with section 7.3 of the R-Codes Vol. 1, the below provisions amend or replace deemed to comply provisions in the R-Codes Vol. 1.

Local Housing Objective (HO)

Housing Objectives provides additional direction for applicants to ensure that proposals are contextually appropriate for the specific sub-precinct.

Lot boundary setbacks - (Note: Awaiting approval from WAPC for b) to apply)

- a) Lot boundary walls are permitted on the southern parent lot boundary (for R40, R60 and R160 lots oriented east/west) and the western parent lot boundary (for R60 lots oriented north/south) to provide a useable consolidated area of open space on one side of the site.
- b) ~~Outdoor living areas are encouraged to be located within the street setback area, subject to:~~
 - i. ~~Deep soil area's also being incorporated within the street setback area; and~~
 - ii. ~~Any visually permeable front fence meeting Part 5.2.4 of the R-Codes Vol. 1 (unless a half basement is provided); and~~
 - iii. ~~Providing a level change between the outdoor living area and the street level. A maximum level difference of 1.2m may be supported for this purpose.~~

Landscaping - (Note: Awaiting approval from WAPC for Deemed-to-Comply provisions to apply)

- a) ~~Any new development is to include the following minimum number of trees (per parent lot or amalgamated parent lots being developed):~~

Site Area	Minimum requirement for trees
<700m ²	2 medium trees AND small trees to suit area
700 – 1000m ²	3 medium trees AND Small trees to suit area OR 1 large tree AND small trees to suit area
>1000m ²	In addition to the above; 1 large tree OR 2 medium trees OR 5 small trees for each additional 400m ² in excess of 1000m ² .

~~b) Of the trees required above, a minimum of 1 medium tree or 1 large tree is required to be planted and maintained in each street setback area for each street facing dwelling, and each rear setback area (either within private or communal open space) for each laneway-facing dwelling. Small trees are required to be provided for internal lots or central areas of the site.~~

~~c) Where a medium tree or large tree is proposed within the street setback area, the minimum tree planting area for that tree may project into the verge by 40%. **Deep soil area** is to be on site and may not project into the verge.~~

~~d) A minimum total of 20% of the site area (area of parent lot/s) is to be provided as **landscaping**. This shall include at least 15% of the site area to be **deep soil area**.~~

~~e) The required **deep soil area** may be reduced to 10% of the site area if a **significant existing tree** is retained on site, or if a large tree is planted on site.~~

5.2 MIXED USE ZONE SUB-PRECINCT

5.2.1 Character Statements

A- Hampden Road (R-AC3)

Existing Character

The Hampden Road sub-precinct is located at the eastern edge of the Precinct and adjoins the City of Perth municipality. Hampden Road is oriented north to south and functions as a retail main street, with smaller office buildings dispersed throughout. Hampden Road provides a key connection between Stirling Highway, Hollywood Hospital and QEII Medical Centre, and is serviced by high frequency Transperth buses.

The northern portion of Hampden Road features a cluster of low-rise office and retail buildings that form the start of a small main street precinct. The central interface with Hampden Road is characterised by low-rise, fine-grain traditional shop front development. Buildings built to the boundary (no side setbacks) create a continuous and consistent edge. Nil front setbacks with awnings enable an engaging street frontage but provide limited opportunity for any trees at the front of buildings. Some buildings are set back, with others contributing awnings and dining spaces to the street. Some landscaped planter boxes are provided as part of small alfresco dining spaces within the road reserve, and a collection of significant trees adjacent to the Lot 276 (No.15) Hampden Road car park shades a portion of the street. The northern portion of Hampden Road comprises a residential edge that connects to QEII Medical Centre.

There are limited through-links between Hampden Road and Hampden Lane, reducing any opportunity for laneway activation. Rear lane access to commercial tenancies is common. Parallel car parking bays within the road reserve provide some separation between the footpath and traffic.

Desired Future Character

Hampden Road can become one of Perth's great neighbourhood streets, one that is centred around the community. Future development shall enhance Hampden Road, creating a unique and vibrant place to live, work, recreate and visit. Hampden Road's north to south orientation is conducive to optimal urban design outcomes. Accordingly, street design, new development, public spaces, and businesses should contribute to an enjoyable experience for everyone. This will create a lively gathering place that serves the local neighbourhood, including residents and its workforce.

Hampden Road will continue to be a bustling, mixed-use main street environment, with the sub-precinct accommodating the most intense built form within the Precinct. Podium style developments two-way access is required in accordance with A3.8.6 of the R-Codes Vol. 2. shall be designed to the human scale, while taller elements are encouraged to be set back from the street, with rooftop terraces and gardens at varying levels throughout the development. Alternative methods of reducing and softening building bulk and scale are strongly encouraged.

New development will utilise a palette of high-quality, natural materials, and diverse architectural styles to create a contemporary built form response. New development should address both Hampden Road and Hampden Lane. A uniform rear setback for properties fronting Hampden Lane will maintain the provision of vehicle access and servicing at the rear and will provide opportunities for trees and **deep soil areas**. The existing mature trees on Lot 276 (No.15) Hampden Road should be retained and incorporated as part of a landscaped alcove or alfresco area. On-structure planting will add greenery and soften any built form. Pedestrian linkages from Hampden Road to Hampden Lane will promote activation, interests, **landscaping** and improve connectivity within the Precinct.

B - Monash Avenue (R-AC3)

Sub-Precinct Existing Character

The Monash Avenue sub-precinct is located at the northern edge of the Precinct. Monash Avenue provides a key connection between Smyth Road to the west and Winthrop Avenue to the east, with Hollywood Hospital and QEII Medical Centre located in between. Monash Avenue provides a residential edge to the precinct, with intermittent medical professional services located within the former residential dwellings. Micrantha Lane is located directly to the south, providing opportunities for rear vehicle access. Houses are generally single storey in height and are an example of a largely intact Inter-War streetscape of modest, well-maintained dwellings, lined with mature street trees.

Monash Avenue (between Williams Road and Clifton Street) comprises a large proportion of California Bungalows with a repetitive asymmetrical form. Dwellings are predominantly masonry, commonly with tuck pointed brick to sill height and rough rendering above. Prominent architectural details in the Character Area include tapered columns, exposed rafters, brick arches and recessed entries. Most properties in this section of Monash Avenue have visually permeable fences, ranging from timber picket fences to hedging. Most properties have open driveways, with some new developments having garages that address the street.

The section of Monash Avenue between Clifton Street and Hampden Road differs to the road's western end. The QEII campus and Hollywood Private Hospital are situated on the opposite side of Monash Avenue, and this influences the area with most properties having high perimeter fences. There is evidence of alterations to Inter-War dwellings, with many roofs now consisting of contemporary corrugated steel in place of traditional terracotta tiles. The area is predominately residential, however there is a new build at the corner of Monash Avenue and Hampden Road that is utilised for commercial activities.

Sub-Precinct Desired Future Character

Future development of Monash Avenue should reflect its location and function in the context of Hollywood Hospital and QEII Medical Centre, with new development to address Monash Avenue. New mixed-use development with less active ground floor uses and medical related uses are anticipated along Monash Avenue to support Hollywood Hospital and QEII Medical Centre. A diversity of dwelling types which provide for a range of household configurations is encouraged on the upper floors. New development will be massed towards Monash Avenue to minimise building bulk impacts on the R60 properties south of Micrantha Lane.

The street interface of new developments will contribute to pedestrian comfort and will provide opportunity for passive surveillance of the streetscape. A rear setback will encourage vehicle access, car parking and servicing to be provided at the rear. A setback will also provide opportunities for **landscaping**, to soften the higher density development fronting Monash Avenue from the R60 lots south of Micrantha Lane.

C - Leura Street (R-AC3)

Existing Character

Leura Street comprises a diverse range of land uses and activities of varying urban forms, including various health and medical services. Single residential dwellings are generally located on its western side and the southern portion of the eastern side, with commercial uses and grouped dwellings located to the north. Leura Street functions as a street of transition between the commercial development to the east and the residential dwellings to the west.

Desired Future Character

Future development on Leura Street should reflect its proximity to Hampden Road and R60 lots to the west, with new development to address Leura Street and Hampden Lane. New mixed-use development with less active ground floor uses and medical related uses are anticipated along Leura Street to support Hollywood Hospital and QEII Medical Centre, while being respectful of residential dwellings to the west. A diversity of dwelling types which provide for a range of household configurations is encouraged. Podium style development will seek to minimise building bulk impacts on the R60 properties on the western side of Leura Street.

The street interface of new developments will provide opportunity for passive surveillance of the streetscape and **deep soil area** to accommodate growth of trees, softening the higher density development from the R60 lots west of Leura Street.

5.2.2 ACCEPTABLE OUTCOMES

Acceptable Outcomes are likely to assist in satisfying the Objectives but are not a comprehensive 'deemed-to-comply' list. To achieve the outcomes described within the Desired Future Character Statements, proposals may require additional and/or alternative design solutions in response to the site conditions, streetscape and design approach. Guidance is provided within the applicable Design Guidance section.

Hampden Road (R-AC3)

AO	Primary Control	Acceptable Outcome
AO 4.1	Building Height ¹	Maximum 6 storeys (21m)
AO 4.2	Primary Street Setback ^{2,4,6}	<u>Ground to 2nd storey</u> Nil <u>3rd, 4th and 5th storeys</u> Minimum 4m <u>6th storey</u> Minimum 6m
AO 4.3	Secondary Street setback ^{2,6}	Minimum 1.5m (all storeys)
AO 4.4	Side setbacks ^{2,5}	<u>Ground to 3rd storey</u> Minimum nil <u>4th storey and above</u> Minimum 3m
AO 4.5	Rear setback ²	Minimum 3m (all storeys)
AO 4.6	Lot Boundary Walls ³	<ul style="list-style-type: none"> • Maximum 3 storeys (10m) in height⁷ • Permitted to two side boundaries

Monash Avenue (R-AC3)

AO	Primary Control	Acceptable Outcome
AO 5.1	Building Height ¹	Maximum 6 storeys (21m)
AO 5.2	Primary Street Setback ^{2,6}	<u>Ground to 2nd storey</u> Nil <u>3rd, 4th and 5th storeys</u> Minimum 4m <u>6th storey</u> Minimum 6m
AO 5.3	Secondary Street setback ^{2,6}	<u>Ground to 2nd storey</u> Minimum 1.5m <u>3rd storey and above</u> Minimum 3m
AO 5.4	Side setbacks ^{2,5}	<u>Ground to 3rd storey</u> Minimum nil <u>4th storey and above</u> Minimum 3m
AO 5.5	Rear setback ²	<u>Up to 5th storey</u> Minimum 3m <u>6th storey</u> Minimum 4.5m
AO 5.6	Lot Boundary Walls ³	<ul style="list-style-type: none"> • Maximum 3 storeys (10m) in height⁷ • Permitted to two side boundaries

Leura Street (R-AC3)

Draft Hampden-Hollywood Precinct Design Response - Local Planning Policy

AO	Primary Control	Acceptable Outcome
AO 6.1	Building Height ¹	Maximum 5 storeys (18m)
AO 6.2	Primary Street Setback ^{2,6}	<u>Ground to 2nd storey</u> Minimum 2m <u>3rd and 4th storeys</u> Minimum 4m <u>5th storey</u> Minimum 6m
AO 6.3	Secondary Street setback ^{2,6}	<u>Ground to 2nd storey</u> Minimum 1.5m <u>3rd storey and above</u> Minimum 3m
AO 6.4	Side setbacks ^{2,5}	<u>Ground to 3rd storey</u> Minimum nil <u>4th storey and above</u> Minimum 3m
AO 6.5	Rear setback ²	Minimum 3m (all storeys)
AO 6.6	Lot Boundary Walls ³	<ul style="list-style-type: none"> • Maximum 3 storeys (10m) in height⁷, • Permitted to two side boundaries

¹ Subject to indicative building heights outlined in Table 2.2 of R-Codes Vol. 2. Maximum overall height inclusive of rooftop articulation.

² Boundary setbacks will also be determined by provisions for building separation and visual privacy within this LPP and building separation provisions of the National Construction Codes.

³ Wall may be built up to a lot boundary, where it abuts an existing or simultaneously constructed wall of equal or greater proportions.

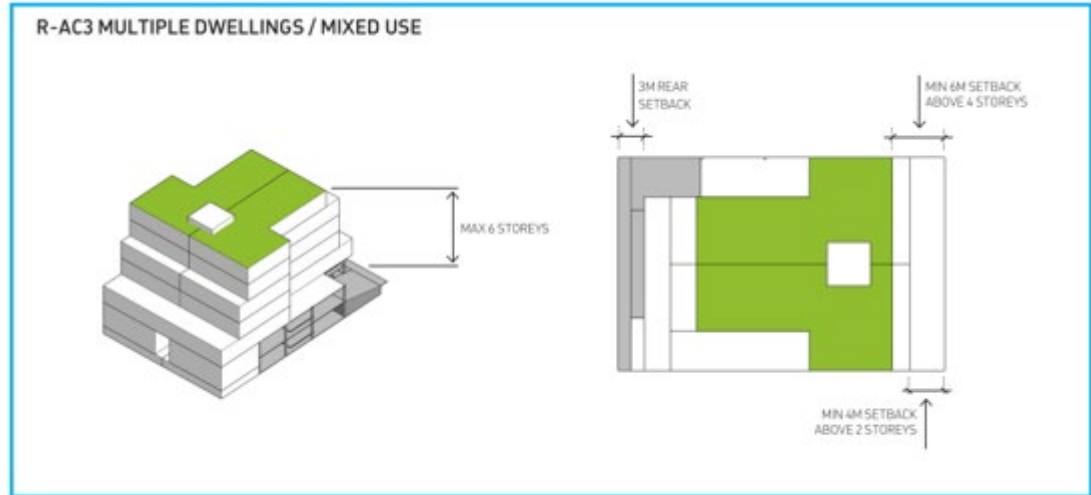
⁴ A Nil Primary Street setback is required to Hampden Road, except when a DSA or landscaped alcove is provided within the setback area.

⁵ Where a side setback greater than Nil is proposed, it must accommodate DSA, activation, or provide a meaningful pedestrian connection to Hampden Lane or Micrantha Lane.

⁶ Residential balconies may project into the street setback area, up to a maximum 2m intrusion, where on-structure planting is incorporated into the balconies which include vegetation visible from the street.

⁷ Subject to indicative height outlined in Table 2.2 of the R-Codes Vol. 2, excluding 2m provision for rooftop articulation.

R-AC3 ZONE



5.2.3 ACCEPTABLE OUTCOMES AND DESIGN GUIDANCE	
<p>Acceptable Outcomes (AO) In accordance with section 1.2.2 and 1.2.3 of the R-Codes Vol. 2, the below provisions amend or replace acceptable outcome provisions in the R-Codes Vol. 2 (refer to Hampden-Hollywood LPP – Relationship to R-Codes).</p>	<p>Design Guidance (DG) Design Guidance provides additional direction for applicants to ensure that proposals are contextually appropriate for the specific sub-precinct.</p>
3.3 Tree canopy and deep soil areas - (Note: Awaiting approval from WAPC for Acceptable Outcomes to apply)	
<p>a) Upper floors of the building may extend over portions of deep soil area, subject to a landscaping plan being provided which demonstrates the selection of appropriate species within the covered area.</p> <p>b) Basement levels are not intrude into the front or rear setback areas, to provide sufficient space for deep soil areas.</p>	<p>a) The retention of the significant existing trees at Lot 276 (No.15) Hampden Road is strongly encouraged, with possible incentives (i.e., variations to R-Code provisions) able to be discussed with the City.</p> <p>b) Deep soil areas and tree plantings should be provided within the rear setback area, creating a landscaped buffer between the proposed development and the adjoining lower density site/s.</p> <p>c) Where hard surfaces are proposed within outdoor living areas, permeable finishes such as gravel or permeable paving should be considered in order to reduce stormwater run-off.</p> <p>d) Deep soil areas should be consolidated as much as possible, resulting in larger, more useable spaces, rather than multiple smaller areas.</p>
3.4 Communal open space	
	<p>a) Communal open space/s should be oriented and screened to maximise northern solar access, while minimising direct overlooking of habitable rooms and private outdoor living areas of adjoining properties.</p> <p>b) The preferred location for communal open space is either:</p> <ol style="list-style-type: none"> i. At upper levels facing Hampden Road or Hampden Lane or Leura Street or Monash Avenue and/or a secondary street; or ii. Co-located with deep soil areas at the rear of the development.
3.8 Vehicle access	
<p>a) Driveway and crossover widths are to be kept to a functional minimal width to maximise space in the street setback area and verge for deep soil areas and tree retention and provision.</p> <p>b) Vehicle access is required to be from Hampden Lane for developments on Hampden Road and Leura Street, and from Micrantha Lane for developments on Monash Avenue, where the laneway is serviceable.</p>	

3.9 Car and bicycle parking - (Note: Awaiting approval from WAPC for Acceptable Outcomes to apply)	
<p>a) Vehicle parking is to be provided at grade or at basement levels, rather than being provided above ground (even if sleeved behind other uses).</p>	
4.10 Façade design	
<p>a) New developments should have an active frontage to Hampden Road, Monash Avenue, Leura Street, and secondary streets where applicable, in accordance with Clause 32.4 (3) of LPS3.</p>	<p>a) The following guidance should be considered to achieve active frontages in the sub-precinct:</p> <p><u>Include</u></p> <ul style="list-style-type: none"> • Variety of façade elements, including articulation, openable windows and doors, stall risers, sills, seating and other fine grain detail. • Variety of materials, colours, textures and depths. • Clearly defined, recessed pedestrian entries. • Landscaping to soften the street interface and provide screening to areas of blank façade. • Where required, well integrated and unobtrusive signage. • Security measures, where provided, are to be: <ul style="list-style-type: none"> ○ Located and installed internally behind the glazing; ○ Transparent and visually permeable to allow views inside the building and enable internal light sources to be seen from the street; and ○ High level of passive surveillance to the street. <p><u>Avoid</u></p> <ul style="list-style-type: none"> • Unarticulated floor-to-ceiling glazing. • Heavily tinted glazing which obscures views between the interior and exterior of the building (any tinting should have a minimum of 70% visible light transmission). • Blank, monotonous facades. • Facades dominated by vehicle entries, parking areas or services/utilities (whether screened or unscreened). • Facades dominated by advertising signage.

4.14 Mixed Use - (Note: Awaiting approval from WAPC for Acceptable Outcomes to apply)

- a) ~~Active land use/s~~ are to be provided at ground level, facing Hampden Road and Monash Avenue. ~~More non-active commercial uses and/or residential uses are preferred on Leura Street.~~
- b) ~~Any non-active land uses are to be located above the ground floor, with residential land uses such as multiple dwellings (apartments) preferred.~~
- c) ~~Ground floor tenancies are to be designed to allow for ease of change to other non-residential uses, including those requiring grease traps and exhaust fans.~~
- d) ~~The combined net lettable area (NLA) of commercial ground floor spaces is to meet the requirements set out in the table below:~~

Development Site	Minimum NLA
Single lot	70m ²
Two or more adjoining lots	60m ² per lot

- e) ~~The above NLA requirements are in addition to the minimum tenancy depth of 10m, as per Clause 32.4 (4) of Local Planning Scheme No. 3.~~

- a) The following land uses are preferred within the Mixed Use sub-precincts:

Hampden Road

- Consulting rooms
- Convenience store
- Liquor store – small
- Lunch bar
- Recreation – private
- Residential
- Restaurant/café
- Serviced apartment
- Shop
- Small bar

Monash Avenue

- Consulting rooms
- Convenience store
- Lunch bar
- Medical centre
- Office
- Recreation – private
- Residential
- Restaurant/café
- Serviced apartment
- Shop

Leura Street

- Consulting rooms
- Medical centre
- Office
- Residential
- Serviced apartment

6 Definitions

Terms used in this Policy are defined as per *State Planning Policy 7.3 Residential Design Codes Volume 1 and Volume 2*, unless otherwise specified below:

Active land use: Land uses which generate many visits, in particular pedestrian visits, over an extended period of the day. Active land uses provide opportunity for spontaneous encounters between neighbours and visitors to the precinct.

Active frontage: Street frontages where there is an active visual engagement between those in the street and those on the ground and upper floors of buildings. Active frontages can provide informal surveillance opportunities and often improve the vitality and safety of an area.

Deep Soil Area: Soft landscape area on lot with no impeding building structure or feature above or below, which supports growth of medium to large canopy trees and meets a stated minimum dimension. Used primarily for landscaping and open to the sky, deep soil areas exclude basement car parks, services, swimming pools, tennis courts and impervious surfaces including car parks, driveways and roof areas.

Landscape Area / Landscaping: Land developed with garden beds, shrubs and trees, or by the planting of lawns, and includes such features as rockeries, ornamental ponds and any other such area approved of by the decision-maker as landscaped area.

Significant existing tree: an existing tree that meets the following criteria:

- healthy specimens with ongoing viability; and
- species is not included on a State or local area weed register; and
- height of at least 4m; and/or
- trunk diameter of at least 160mm, measured 1m from the ground; and/or
- average canopy diameter of at least 4m.

Appendix 1 – Sustainability and Energy Efficiency Initiatives

Examples of energy efficient initiatives that exceed current minimum practice are provided below. Applicants are encouraged to propose other innovative solutions where supported by evidence demonstrating how minimum practice is exceeded:

- Ceiling fans to all habitable rooms
- Hot water systems that are more energy efficient than electric storage units
- Provision of an external clothesline to every dwelling, located in a secure area, out of direct view on an external wall or in a breezeway
- Use of a photovoltaic array for communal services
- Installation of a lift with regenerative braking
- Solar powered lighting of external open space, circulation areas and common spaces
- Battery backup for PV generated power
- Embedded network for power distribution
- Grey-water recycling
- On-site green waste recycling and/or power generation
- Electric vehicle charging
- Shared electric vehicles
- Rainwater harvesting

Appendix 2 – Facades and Materials

Residential Sub-Precinct

Key Elements

- | | |
|---|--|
| <ul style="list-style-type: none"> • High quality materials and finishes which enhance the existing character of the precinct. • Natural warm-toned materials including rendered masonry, face brick, stone, timber and contemporary cladding. • Articulated forms with vertical proportions. • Blank facades to be avoided. • Low style front fencing | <ul style="list-style-type: none"> • Verandahs, eaves, louvers, balconies, window awnings and hoods reinterpreted in a contemporary manner. • Façade design elements should enhance the solar passive design of all the elements. • Boundary walls to be finished to same quality as the elevation to the primary street where it faces a neighbouring lot. |
|---|--|

Examples of key elements within the sub-precinct and surrounds



Examples of key elements in new developments

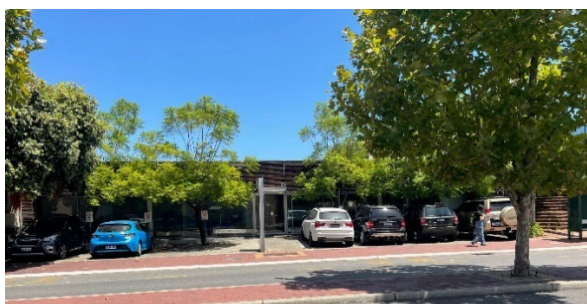


Mixed Use Sub-Precinct

Key Elements

- High quality materials and finishes which enhance the existing character of the precinct.
- Natural warm-toned materials including rendered masonry, face brick, stone, timber and contemporary cladding.
- Verandahs, eaves, louvers, balconies, window awnings and hoods reinterpreted in a contemporary manner.
- Articulated forms with vertical proportions.
- Openable windows and doors, stall risers, sills, seating and other fine grain detail to create visual interest.
- Incorporation of **landscaping** to soften built form.
- Use of glazing to allow for visual interaction between building and street.
- Upper floors to address street and provide for passive surveillance.
- Façade design elements should enhance the solar passive design of all the elements.
- Boundary walls to be finished to same quality as the elevation to the primary street where it faces a neighbouring lot.
- Blank facades to be avoided.

Examples of key elements within the sub-precinct and surrounds



Appendix 3 - Parking Requirements - Mixed Use Zone (Preferred Active Land Uses)

1. For any, at grade, uncovered carparks, a minimum of one medium tree for every four bays or one large tree for every 6 bays.
2. Car parking is to be in accordance with the rates specified within Table 1 below. Where a land use is not listed, is it to be in accordance with the City's *Local Planning Policy - Parking*.
3. The City will consider a reduction in the provision of visitor parking in instances where:
 - i. a **significant existing tree** is retained in the front setback area;
OR
 - ii. the development achieves or exceeds the required **deep soil area**, tree canopy and **landscaping** provisions of this Policy and R-codes to the satisfaction of the City **Table 1 – Parking requirements - Mixed Use zone (Preferred Active Land Uses)**

Land use	Minimum number of car parking bays required	Minimum number of bicycle bays required
Convenience store Liquor store – small Restaurant / café Shop	1 car bay per 30m ² NLA	1 per 30m ² of NLA
Recreation – private	1 car bay per 20m ² NLA	1 per 50m ² of NLA
Small bar Tavern	1 car bay per 20m ² of NLA (inclusive of dining areas and function rooms)	1 per 30m ² of NLA
Consulting rooms Medical centre	3 car bays for each practitioner In addition to the above, 2 car bays for each practitioner in excess of 2 practitioners	1 per 10 car parking bays
Office	1 per 40m ² NLA	1 per 200m ² of NLA
<p><i>Notes:</i></p> <ul style="list-style-type: none"> • <i>NLA = net lettable area</i> • <i>The number of parking spaces calculated shall be rounded to the nearest whole number.</i> • <i>The required bicycle spaces are to be clearly visible and directly accessible from the Primary Street.</i> • <i>Bicycle spaces should be provided in convenient, secure and undercover locations.</i> 		

Council Resolution Number	PD10.03.23
Adoption Date	OCM 28 March 2023
Date Reviewed/Modified	